



Santa Cruz County
Regional Transportation Commission's
Elderly & Disabled
Transportation Advisory Committee
Social Service Transportation Advisory Council

MEETING AGENDA

Tuesday, June 9, 2026

1:30 – 3:30pm

In-Person Meeting

1101 Pacific Avenue, Suite 250,
Santa Cruz, CA 95060

REMOTE PARTICIPATION

(see end of agenda for more information)

<https://us02web.zoom.us/j/82217044415>

Meeting ID: 822 1704 4415

Dial by your location: +1 669 900 9128

Accessibility: See end of agenda for details.

En Español: Para servicios de traducción al español, diríjase a la última página.

Agendas Online: www.sccrtc.org/meetings/elderly-disabled/

1. 1:30pm — Call to Order
2. 1:30pm — Introductions
3. 1:32pm — Consider AB2449 request(s) to participate in the meeting remotely due to emergency circumstances (a physical or family medical emergency that prevents a member from attending in person)
4. 1:35pm — Oral communications
5. 1:40pm — Additions or deletions to the consent or regular agenda

CONSENT AGENDA

All items appearing on the consent agenda are considered to be minor or non-controversial and will be acted upon in one motion if no member of the E&D TAC or public wishes an item be removed and discussed on the regular agenda. Members of the E&D TAC may raise questions, seek clarification or add directions to Consent Agenda items without removing the item from the Consent Agenda as long as no other E&D TAC member objects to the change.

6. Receive information items – pg. 6

- a. E&D TAC Letter of Support for the City of Capitola’s Safe Streets and Roads for All (SS4A) Implementation Grant Application

7. Receive RTC Meeting Highlights from the May 7 meeting – pg. 8

REGULAR AGENDA

8. Planning for Climate Resilience of the Santa Cruz Branch Rail Line Milestone 2 Study Update – pg. 11

9. City of Watsonville Clifford Avenue Road Rehabilitation and Traffic Calming Project – pg. 24

10. Cruz 511 Hazard Report Form – pg. 32

11. Receive Program Updates – pg. 42

- a. Volunteer Center
- b. Community Bridges
- c. Santa Cruz Metro
- d. SCCRTC
- e. Pedestrian Ad-hoc Subcommittee
 - i. Pedestrian Hazard Report

Adjourn – 3:30 pm

NEXT MEETING: Tuesday August 11, 2026, at 1:30pm hosted in person at the SCCRTC office located at 1101 Pacific Avenue, Suite 250, Santa Cruz, CA 95060.

HOW TO REACH US

Santa Cruz County Regional Transportation Commission
1101 Pacific Avenue, Suite 250 Santa Cruz, CA 95060
phone: (831) 460-3200 / email: info@sccrtc.org

AGENDA PACKETS

Complete agenda packets and all documents relating to items on the agenda are posted online at www.sccrtc.org/meetings/elderly-disabled/ at least 72 hours prior to the meeting. Sign up for E-News updates at sccrtc.org/about/esubscriptions/

REMOTE PARTICIPATION – Committee Members (AB 2449)

This meeting is being held in accordance with the California Ralph M. Brown Act as amended by AB2449 of 2022 and AB2302 of 2024 and as interpreted by Attorney General Opinion 23-1002.

- 1. Members of the committee may attend by teleconference if the location from which they are attending is open to the public to participate and the remote meeting location is listed on the agenda.*
- 2. Members of the committee may attend via zoom up to two times per year due to an emergency or for cause according to requirements set forth in Government Code Section 54953, as long as a quorum of the committee is present in person at one meeting location within the county. The remote location from which the member is participating does not need to be listed on the agenda and does not need to be available to the public.*
 - a. Government Code Section 54953(j) defines "just cause" as:*
 - i. Care of a child, parent, grandparent, grandchild, sibling, spouse, or domestic partner;*
 - ii. a contagious illness that prevents a member from attending in person;*
 - iii. a need related to a physical or mental disability as defined by statute; or*
 - iv. travel while on official business of the RTC or another state or local agency*
 - b. Government Code Section 54953(j) defines "emergency circumstances" as a physical or family medical emergency that prevents a member from attending in person. The committee member must provide a general description of the circumstances relating to your need to appear remotely at the given meeting (not exceeding 20*

words). Medical condition does not need to be disclosed. The E&D TAC must take action to approve the request to participate due to an emergency circumstance at the start of their regularly scheduled meeting.

3. Per Attorney General Opinion 23-1002, members with an Americans with Disabilities Act (ADA) qualifying disability that precludes their in-person attendance may participate remotely as a reasonable accommodation due to their disability.
4. **Under any circumstance that a member is participating remotely:** The members must be connected in real time through both audio and visual means, and they must disclose the identities of any adults present with them at the remote location.

REMOTE PARTICIPATION – Public

The public may participate in the meetings of the Regional Transportation Commission (RTC) and its committees in person or remotely via the provided Zoom link. If technical difficulties result in the loss of communication for remote participants, the RTC will work to restore the communication; however, the meeting will continue while efforts are being made to restore communication to the remote participants. Members of the public participating by Zoom are instructed to be on mute during the proceedings and to speak only when public comment is allowed, after requesting and receiving recognition from the Chair.

PARTICIPACIÓN REMOTA – El público

El público puede participar en las juntas de la Comisión Regional de Transporte (RTC) en persona o remotamente a través del enlace Zoom proporcionado. Si problemas técnicos resultan en la pérdida de comunicación con quienes participan remotamente, la RTC hará lo posible por restaurar la comunicación. Pero, la junta continuara mientras se hace lo posible por restaurar la comunicación con quienes participan remotamente

ACCOMIDATIONS FOR PEOPLE WITH DISABILITIES

The Santa Cruz County Regional Transportation Commission does not discriminate on the basis of disability and no person shall, by reason of a disability, be denied the benefits of its services, programs, or activities. This meeting location is an accessible facility. If you wish to attend this meeting and require special assistance in order to participate, please contact RTC staff at 460-3200 (CRS 800/735-2929) at least three working days in advance of this meeting to make arrangements. People with disabilities may request a copy of the agenda in an alternative format. As a courtesy to those persons affected, please attend the meeting smoke and scent-free.

SERVICIOS DE TRADUCCIÓN/TRANSLATION SERVICES

Si gusta estar presente o participar en esta junta de la Comisión Regional de Transporte del condado de Santa Cruz y necesita información o servicios de traducción al español por favor llame por lo menos con tres días laborales de anticipo al (831) 460-3200 para hacer los arreglos necesarios.

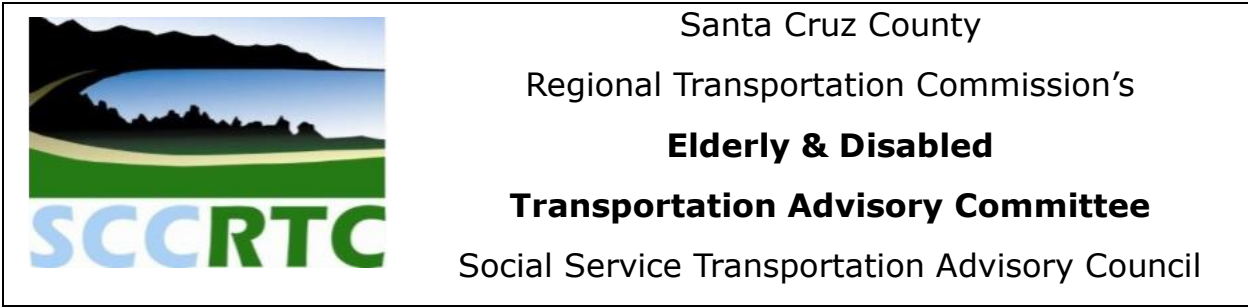
Spanish language translation is available on an as needed basis. Please make advance arrangements at least three days in advance by calling (831) 460-3200.

TITLE VI NOTICE

The RTC operates its programs and services without regard to race, color and national origin in accordance with Title VI of the Civil Rights Act. Any person believing to have been aggrieved by the RTC under Title VI may file a complaint with RTC by contacting the RTC at (831) 460-3200 or 1101 Pacific Avenue Suite 250, Santa Cruz, CA, 95060 or online at www.sccrtc.org. A complaint may also be filed directly with the Federal Transit Administration to the Office of Civil Rights, Attention: Title VI Program Coordinator, East Building, 5th Floor-TCR, 1200 New Jersey Ave., SE, Washington, DC 20590.

AVISO A BENEFICIARIOS SOBRE EL TITULO VI

La RTC conduce sus programas y otorga sus servicios sin considerar raza, color u origen nacional de acuerdo al Título VI del Acta Sobre los Derechos Civiles. Cualquier persona que cree haber sido ofendida por la RTC bajo el Título VI puede entregar queja con la RTC comunicándose al (831) 460-3200 o 1101 Pacific Avenue, Suite 250, Santa Cruz, CA 95060 o en línea al www.sccrtc.org. También se puede quejar directamente con la Administración Federal de Transporte en la Oficina de Derechos Civiles, Atención: Coordinador del Programa Título VI, East Building, 5th Floor-TCR, 1200 New Jersey Avenue, SE, Washington, DC 20590.



May 22, 2026

U.S. Department of Transportation (USDOT)
Attn: Office of Infrastructure Finance and Innovation
1200 New Jersey Avenue, SE
Washington, DC 20590

RE: Support for the City of Capitola’s Safe Streets and Roads for All (SS4A) Implementation Grant Application

The Elderly & Disabled Transportation Advisory Committee (E&D TAC) advises the Santa Cruz County Regional Transportation Commission (RTC), the Santa Cruz Metropolitan Transit District (Metro), and other service providers on transportation needs for people with disabilities, seniors and persons of limited means.

On behalf of The RTC’s Elderly and Disabled Transportation Advisory Committee, I am writing to express our strong support for the City of Capitola’s Safe Streets and Roads for All (SS4A) Implementation Grant Application. This funding opportunity will allow the City to advance citywide safety improvements identified in its recently completed Local Roadway Safety Plan (LRSP) and reduce fatal and serious injury crashes across Capitola’s transportation network.

Roadway safety is a shared priority for our organization and the City of Capitola. The proposed SS4A program applies a systemic, data driven approach to improving safety at priority intersections, corridors, and roadway segments with documented histories of pedestrian, bicycle, and multimodal crashes. Proven, low-cost countermeasures like enhanced pedestrian crossings, RRFBs, LPIs, lighting upgrades, improved signage, and speed management will address high-risk locations citywide. In conjunction with our committee, many of the safety improvements focus attention on

addressing the needs of pedestrians living with disabilities who benefit from solutions ranging from longer crossing times to accessible pedestrian signals, reduced crossing distances or increased visibility.

In addition, the City's application includes supplemental planning and demonstration activities that will further enhance pedestrian and bicycle safety citywide, especially within the vicinity of schools. These efforts include implementation of safety measures identified in City's Complete Street Safety Assessment study, traffic safety audits for select intersections and roadway segments, and targeted community outreach focusing on education for and about vulnerable roadway users. Together, these activities will support equitable engagement, inform future project development, and ensure safety investments remain aligned with community priorities and the Safe System Approach.

As a partner in the City's traffic safety efforts, our committee believes the SS4A implementation grant is a vital opportunity for the City to achieve its safety goals. By implementing systemic countermeasures throughout the City, conducting traffic safety audits, targeted outreach, and demonstration activities, the City will effectively pinpoint critical areas and devise strategic solutions for safety enhancements.

This grant will empower the City of Capitola to make substantial progress in ensuring safer streets for all residents and commuters by responding effectively and quickly to their needs and concerns. We fully support this initiative and urge favorable consideration of this application. Thank you for your leadership in advancing roadway safety nationwide.

Sincerely,



Veronica Elsea

Chair, Santa Cruz County Regional Transportation Commission's
Elderly and Disabled Transportation Advisory Committee

1101 Pacific AVE STE 250
Santa Cruz, CA 95060
831.460.3200



*Santa Cruz County Regional Transportation Commission
1101 Pacific Avenue, Suite 250, Santa Cruz, CA 95060
phone: (831) 460-3200
email: info@scrtc.org; website: www.scrtc.org*

CONTACT: Shannon Munz, Communications Specialist (smunz@scrtc.org)

Santa Cruz County Regional Transportation Commission (RTC) May 7, 2026 Meeting Highlights

Regional Transportation Commission Programming & Budget Amendments and Fiscal Year 2026-27 Transportation Development Act (TDA) Claims

The Commission approved amending the RTC Programming and FY 2025/26 and FY 2026/27 Budget to amend previously programmed projects, as requested by project sponsors, and shift funding to reflect scope and funding updates. The RTC has regularly approved requests from project sponsors to shift funds to other projects that the RTC has approved for funding, in order to help fill funding gaps, cost increases, or integrate additional complete streets elements.

Additionally, the RTC allocates Transportation Development Act (TDA) funds from the region's ¼ cent share of the state's 7.25 cent sales tax and State Transit Assistance (STA) funds, which are generated from the sales tax on diesel fuel. TDA claims for Santa Cruz Metropolitan Transit District, Community Bridges, the Volunteer Center, and RTC administration and planning were approved, which allows recipients to use the funds starting in the next fiscal year.

Authority to Enter into an Administration, Coordination, and License Agreement with Santa Cruz County Coastal Rail for the Remainder of the Freight Easement over the Santa Cruz Branch Rail Line

The Commission approved entering into an Administration, Coordination, and License (ACL) Agreement with Santa Cruz County Coastal Rail (SCCCR). To advance the RTC's goal of utilizing the Santa Cruz Branch Rail Line (SCBRL) for transportation purposes, the RTC created SCCCR as a separate entity to serve as the designated common carrier for the SCBRL. The purpose of SCCCR under the ACL is to preserve the Remainder of the Freight Easement for reactivation of freight service when the repairs to the infrastructure are made and there is a reasonable request for freight shipping service, and to provide functions to manage and maintain the easement property and facilities. Staff anticipates SCCCR will remain the non-operating common carrier for the foreseeable future and will have limited responsibilities. The Commission will retain the rights to advance key public projects, like the Coastal Rail Trail and Passenger Rail, within the corridor so long as they do not permanently interfere with freight use.

2026 Draft Unmet Transit and Paratransit Needs List

Following a public hearing, the Commission adopted the [2026 Unmet Transit and Paratransit Needs List](#). Unmet transit and paratransit needs are those transportation needs which are not being met by the current public transit system, have community support, and do not duplicate transit services provided publicly or privately. The Commission solicited input from the public and the RTC's Elderly and Disabled Transportation Advisory Committee (E&D TAC), partner agencies and other local organizations to identify unmet transit needs of transit-dependent or disadvantaged persons. The RTC, Santa Cruz METRO and other community transportation service providers will consider unmet paratransit and transit needs from the list as funding becomes available.

Upcoming RTC and Committee Meetings

Regional Transportation Commission Meeting

Thursday, June 11, 2026, 9:00 a.m.

Bicycle Advisory Committee

Monday, May 11, 2026, 5:30 p.m.

Elderly & Disabled Transportation Advisory Committee

Tuesday, May 12, 2026, 1:30 p.m.

Interagency Technical Advisory Committee

Thursday, May 21, 2026, 1:30 p.m.

RTC and committee meetings are held in person. Non-voting members of the Commission and its committees, as well as members of the public and staff, will have the option to participate in person or remotely, provided equipment is available at the meeting location to allow remote participation. If there are technical difficulties during a meeting that prevent remote participation, the meeting will continue. Please check the RTC website [<https://sccrtc.org/meetings/calendar/>] or call 460-3200 to confirm meeting and video conference information for future meetings. Agendas are posted to the website at least 3 days before the meeting and will also include attendance information. Meetings may be canceled if there are no action items to be considered by the committee.

The RTC is committed to its compliance with the Americans with Disabilities Act (ADA). Please contact the RTC at least 3 days in advance of a meeting if special accommodations are needed. If any document, webpage, meeting, or recording is inaccessible to you, kindly notify us at info@sccrtc.org or by calling 831-460-3200.

Public input on transportation issues is welcomed and encouraged. For more information, visit the SCCRTC website at www.sccrtc.org or call 460-3200. Some Regional Transportation Commission meetings are televised countywide by Community TV of Santa Cruz. Consult www.communitytv.org or call 831-425-8848 for schedule and station Information.

TO: Regional Transportation Commission Technical Advisory Committees

FROM: Amanda Marino, Transportation Planner

RE: Planning for Climate Resilience of the Santa Cruz Branch Rail Line Milestone 2 Study Update

RECOMMENDATION

Staff recommends that the RTC Bicycle Advisory Committee, Elderly and Disabled Transportation Advisory Committee, and Interagency Technical Advisory Committee receive information on the Planning for Climate Resilience of the Santa Cruz Branch Rail Line Milestone 2 Study update.

BACKGROUND

The Santa Cruz County Regional Transportation Commission (RTC) purchased the Santa Cruz Branch Rail Line (SCBRL) in 2012 to preserve as a future transportation corridor. This 32-mile corridor, running near the coast from Pajaro Junction to Santa Cruz, is proposed for passenger and freight rail, as well as bicycle and pedestrian trail. Portions of the Coastal Rail Trail have been constructed or are in development along the SCBRL corridor. In December 2025, the RTC took action on the ZEPRT Final Project Concept Report and directed staff to prepare the final design and take actions needed to implement the Interim Trail for Segments 8 through 11 along the SCBRL.

Like many communities in California and nationwide, Santa Cruz County is facing the effects of climate hazards and sea level rise. Storms, rising tides and coastal erosion are expected to intensify, requiring thoughtful planning to ensure the preservation of transportation assets. Funded by a Caltrans Sustainable Planning grant, the RTC initiated a climate resiliency study to understand potential climate hazards impacts and concepts for climate resiliency based on the transportation uses described in the Zero Emission Passenger Rail and Trail Project Concept Report.

In January 2026, RTC approved an amendment to the professional services contract associated with the Climate Resiliency for the ZEPRT Corridor Study to add scope to create concepts for the both the short-term Interim Trail in

addition to the longer-term passenger rail and trail concept on the Santa Cruz Branch Rail Line.

DISCUSSION

RTC is leading a planning study to assess climate hazards impacting the Santa Cruz Branch Rail Line transportation corridor including but not limited to coastal erosion and flooding, to evaluate how these hazards may be exacerbated by sea level rise and climate changes. This information will be used to develop climate resilient transportation facility concepts at the SCBRL corridor's most vulnerable sites over the projected life of projects and based on the adaptability of different types of infrastructure. Four sites are the focus of this study and include

- Capitola Bluffs
- La Selva/Manresa Bluffs
- Harkins Slough Rail Crossing
- Pajaro River Rail Bridge

The concepts will be developed for the use of an interim trail in the short term and combined passenger rail, freight and trail in the longer term at the Capitola Bluffs, La Selva/Manresa Bluffs, and Harkins Slough Rail Crossing. The climate resiliency concepts at Pajaro River Rail Bridge will focus on passenger rail and freight rail only based on the assumption that a new separate bridge for a trail will be constructed across the Pajaro River and that active freight rail will continue in this area in the short as well as long term.

The first step in the study was to identify the climate hazards at these locations (Milestone 1). The results of the climate hazards analysis inform the climate resilient transportation concepts that could be evaluated moving forward. Some examples of climate resilient concepts that could be contemplated at each of these locations could include rock slope protection, beach nourishment, drainage improvements, and alignment modifications of the rail and trail infrastructure.

Milestone 2 involves developing climate resilient concepts at each location for both a short term and longer term transportation needs. Each concept considers a scenario under which existing facilities are protected or designed to adapt to future conditions. As appropriate, relocation of facilities will also be considered. Then each concept will be evaluated using a standard set of criteria to compare the benefits. The results of the evaluation will inform a selected preferred concept. The evaluation criteria include measures such as resilience to climate hazards, cost, environmental impacts, public access,

active transportation benefits, and transportation benefits to transit-dependent and underserved populations.

The Draft Climate Resilience Plan will identify preferred concepts that can be incorporated in the project design and project next steps including environmental review. The plan will also incorporate a roadmap for implementation and potential funding sources. The Final Climate Resilience Plan will include a recommendation of a selected preferred design concept for each of the vulnerable sites for both the short and long term use.

Project Schedule:



Upcoming Public Input Opportunities (Milestone 2):

- In-Person Community Workshop: June 9 from 6:00 – 7:30 pm at the Aptos Grange Community Center.
- In-Person Community Workshop: June 10 from 6:00 – 7:30 pm at the Watsonville Civic Plaza Community Room.

To stay up to date with the plan progress visit:

<https://www.sccrtc.org/projects/rail/scbrlclimatestudy/> for updates and to sign up for the study’s mailing list.

Email zeprtCR@sccrtc.org with comments or questions.

Therefore, **Staff recommends that the Regional Transportation Commission Technical Advisory Committees receive information on the Planning for Climate Resilience of the Santa Cruz Branch Rail Line (SCBRL) Milestone 2 Study update.**

FISCAL IMPACT

There are no new fiscal impacts associated with receiving information on the Planning for Climate Resilience of the SCBRL Milestone 2 update. This planning study is funded by a Caltrans Sustainable Transportation Planning grant awarded in 2024 with a match in Measure D and RTC planning funds.

SUMMARY

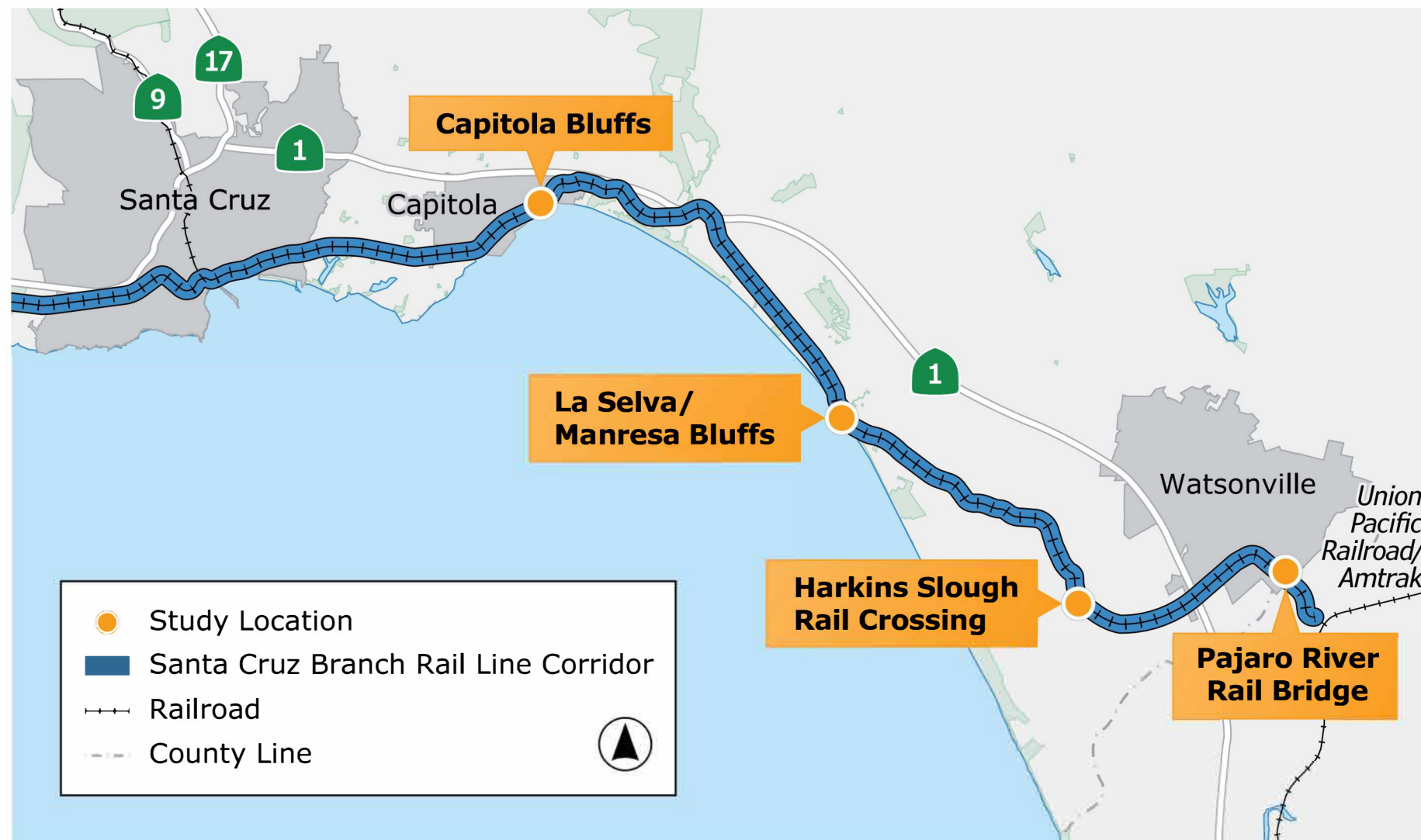
RTC staff was awarded a Caltrans Sustainable Transportation Planning grant to develop climate-resilient concepts for four vulnerable locations along the SCBRL, ensuring the long-term viability of this transportation asset. The current phase of the study, Milestone 2, is focused on providing information on climate hazards and gathering input on the design concept alternatives, and evaluation criteria.

ATTACHMENTS

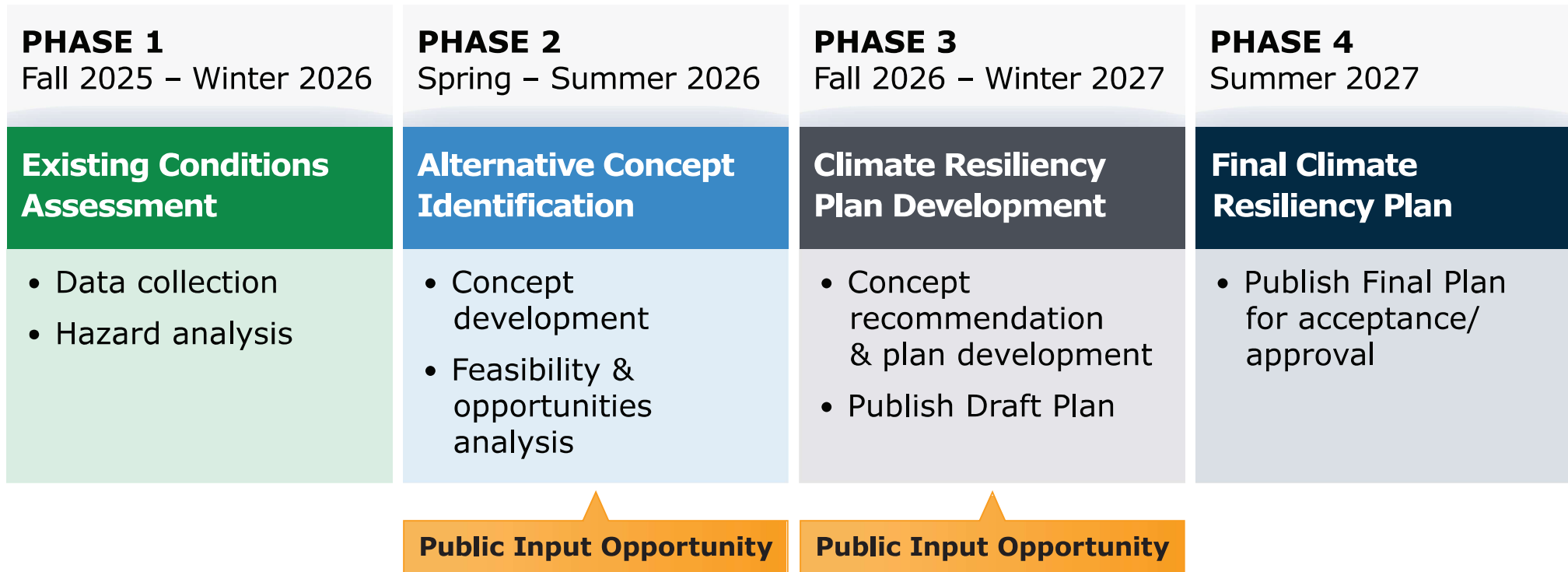
1. Milestone 2 Outreach Materials

What is the Climate Resilience Study?

- **Assesses climate threats** at the four most vulnerable areas of the Santa Cruz Branch Rail Line (SCBRL) corridor
- **Proposes and evaluates concepts** to make the SCBRL corridor more resilient to climate change impacts
- **Considers future transportation uses** of the SCBRL corridor:
 - Interim trail in the short-term
 - Passenger and freight rail alongside trail in the long-term (Zero Emission Passenger Rail and Trail Project)
- **Informs future decision making** of transportation projects at these locations



Climate Resilience Study Schedule



What are the Future Climate Hazards?

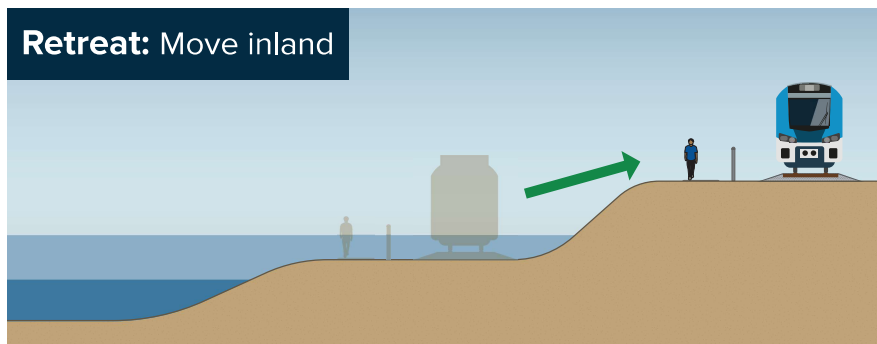
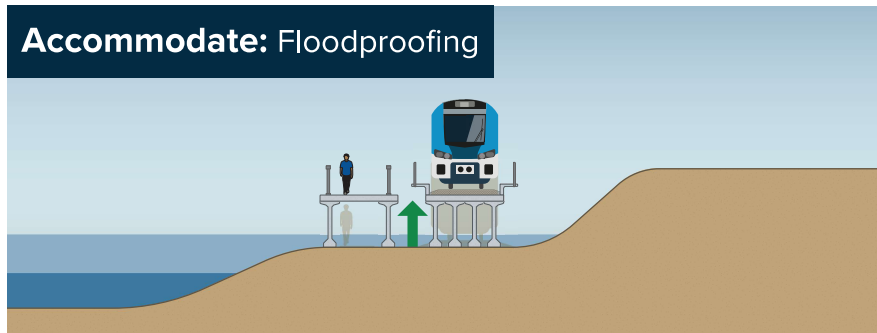
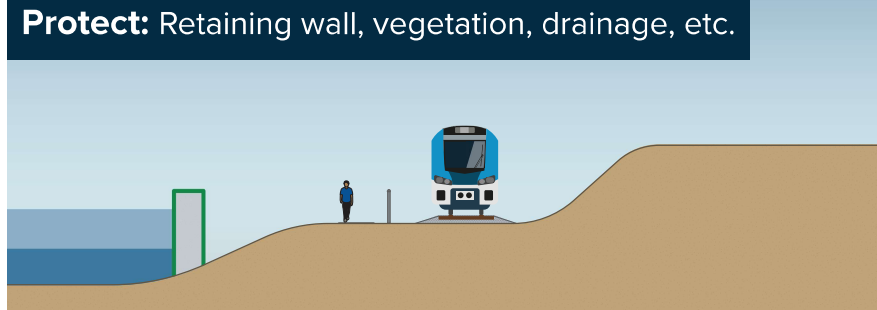
- Stronger storms, rising tides and coastal erosion are expected to intensify, requiring planning to ensure the future of our transportation corridors.
- Study looks at the conditions of the sites today and how they will change over time from the impacts of climate hazards.
- Climate hazards evaluated in this study include sea level rise, increased precipitation, waves and storm surge, coastal erosion, flooding, and wildfires.



Coastal Erosion: Capitola Bluffs photo by City of Capitola. River Flooding: Pajaro River Rail Bridge photo by Josh Edelson/AFP, from Getty Images.

What Concepts are Being Considered?

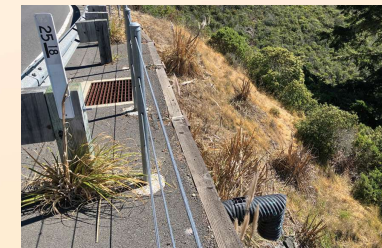
- After understanding the hazards, the next step is to look at a range of options to address hazards at each site.
- The proposed Zero Emission Passenger Rail and Trail (ZEPRT) Final Project Concept rail and trail alignments are the baseline that alternatives will be considered against.
- RTC will consider **Protect/ Accommodate/Retreat alternatives** to make sure a variety of options are considered, from measures that may protect infrastructure for short term and long term use of the corridor.



What is a Concept?

The features or structures that make up how our transportation system is designed and built to improve the resilience of the infrastructure.

Examples



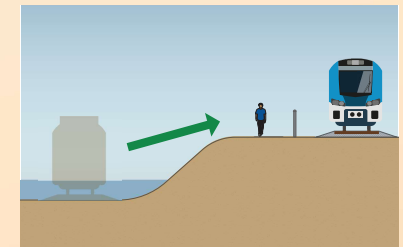
Drainage Improvements



Dune Creation/Revegetation



Viaduct



Relocation

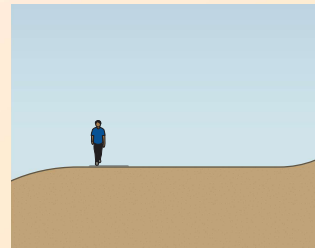
Dune Restoration/Revegetation photo from Manhattan Beach Dune Restoration Project. Viaduct photo by Perry A. Dominguez.

What Timeframe Do We Plan For?

Two Projects Are Being Studied

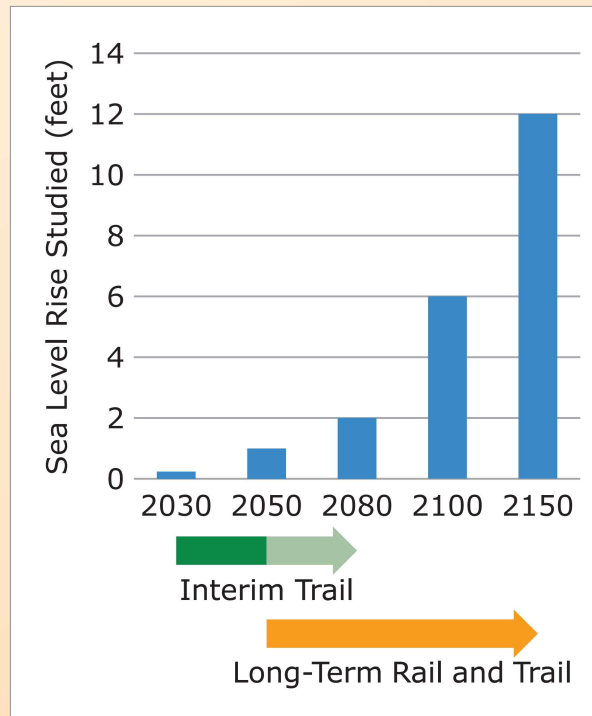
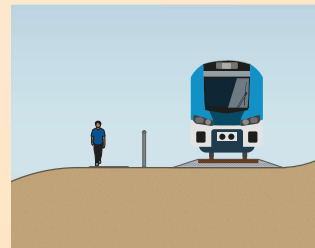
Interim Trail

- Assuming construction in **2030**
- Aiming to be resilient for 50 years (**through 2080**)



Long-Term Rail and Trail

- Assuming construction in **2050**
- Aiming to be resilient for 100 years (**through 20150**)



Concepts Can Vary Based on Project and Timeframe

The hazards vary over time, so some concepts will work better in the short term than in the long term.

The consequences of a temporary loss of service are more severe with a rail than for a trail.

Design and safety standards are different for a trail or a rail line.

Timeframes and phasing of the concepts will be discussed during the study's next Milestone.

Capitola Bluffs

Hazards and Concepts Overview

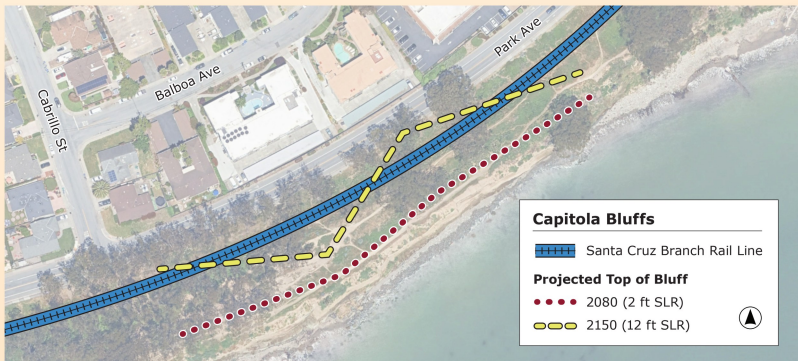
Hazards

What is going to happen?



Existing Conditions

Bluff erosion, mainly caused by waves.



Future Conditions

Sea level rise will cause more waves to hit the face of the bluff, increasing the rate of erosion.

The alignment is projected to be threatened around 2100 (with 6 feet of sea-level rise).

Concepts

What can we do about it?



1 Retaining Wall



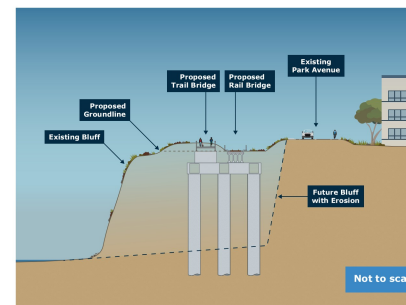
2 Rock Slope Protection



3 Beach Nourishment



4 Sand Retaining Groins



5 Buried Hillside Viaduct

Relocation

- 6 Park Avenue on Viaduct
- 7 Monterey Avenue on Viaduct

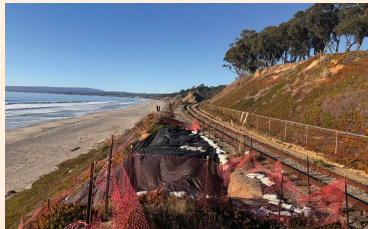
Existing Conditions photo by City of Capitola. Retaining Wall photo by DP Gadal/iStock. Rock Slope Protection photo from Pima Stone, LLC. Beach Nourishment photo by Jim West/Alamy Stock. Sand Retaining Groins photo by Jeff Gritchen, Orange County Register.

La Selva/Manresa Bluffs

Hazards and Concepts Overview

Hazards

What is going to happen?



Existing Conditions

Bluff erosion, mainly caused by rainfall and surface water runoff.



Future Conditions

Current erosion causes would continue, but sea level rise will cause more waves to hit the face of the bluff, significantly increasing the rate of erosion.

Concepts

What can we do about it?



1 Drainage Improvements



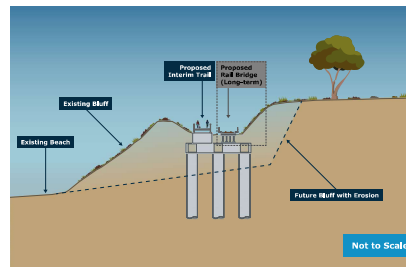
2 Vegetation Restoration

3 Dune Creation

These would appear similar to each other after constructed.



4 Rock Slope Protection



5 Buried Hillside Viaduct

Relocation

- 6 Freight and passenger rail to Highway 1 corridor, connects with SCBRL corridor at Buena Vista Drive
- 7 Freight rail at current alignment, passenger rail following Highway 1 corridor to Watsonville

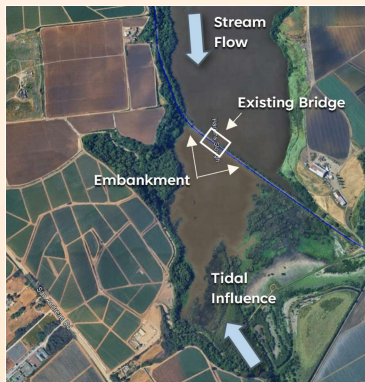
*Vegetation Restoration and Dune Creation photo from Wadulh Dunes Restoration Project.
Rock Slope Protection photo by Carlos Avila Gonzalez.*

Harkins Slough

Hazards and Concepts Overview

Hazards

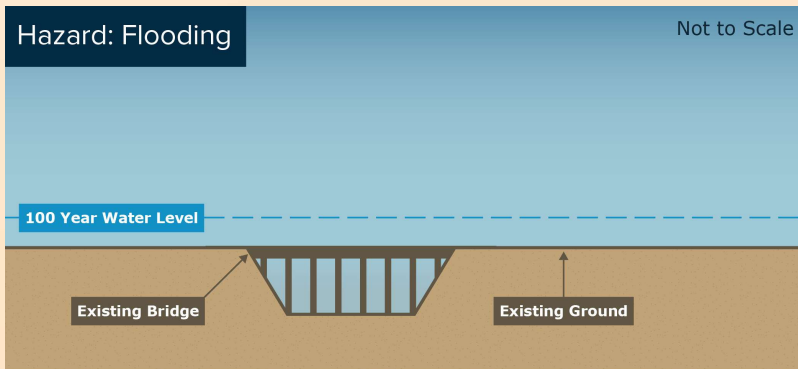
What is going to happen?



Existing and Future Conditions

Flooding overtops the embankment and the bridge in large storms. Currently this is mainly caused by stream flows. Climate change will result in flooding from both tidal backwater and stream flows.

Concepts shown at the location of the existing bridge.



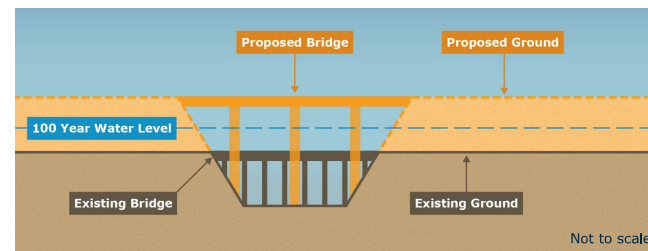
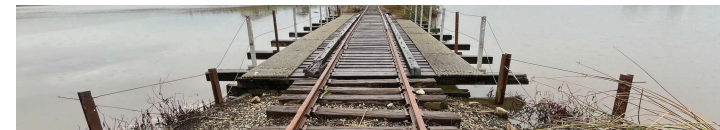
Existing and Future 100-year water levels are similar, so one is shown for simplicity.

Concepts

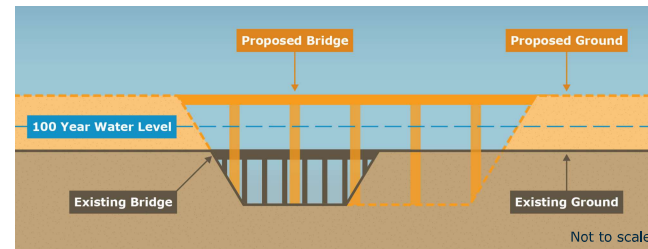
What can we do about it?

1 Rehabilitate/Retrofit Existing Bridge

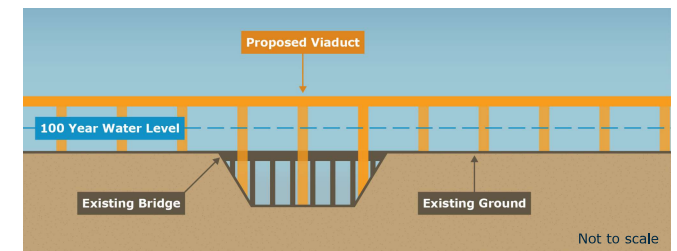
This only applies to interim trail.



2 Elevate Bridge — Same Opening Size



3 Elevate Bridge — Expand Opening



4 Viaduct Across Entire Slough

Relocation

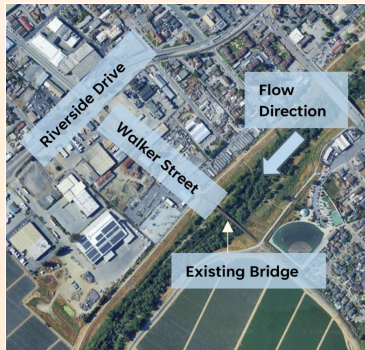
- 5 Freight and passenger rail along San Andreas Road to Watsonville
- 6 Freight rail at current alignment, passenger rail to Highway 1 corridor to Watsonville

Pajaro River

Hazards and Concepts Overview

Hazards

What is going to happen?

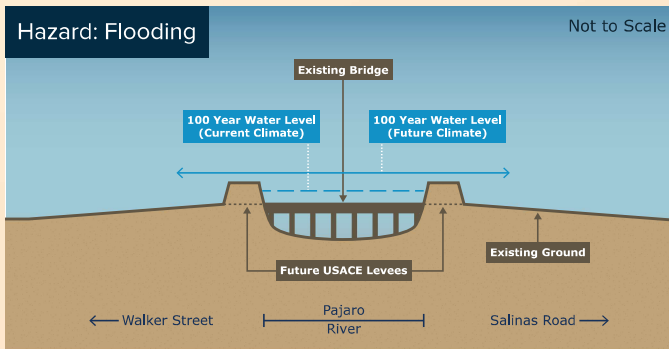


Existing Conditions

The bridge and channel are undersized for the 100-year flow.

Baseline Condition (with USACE Levees)

The water surface is higher but contained within the levees in the channel. The bridge is undersized when considering climate change impacts.

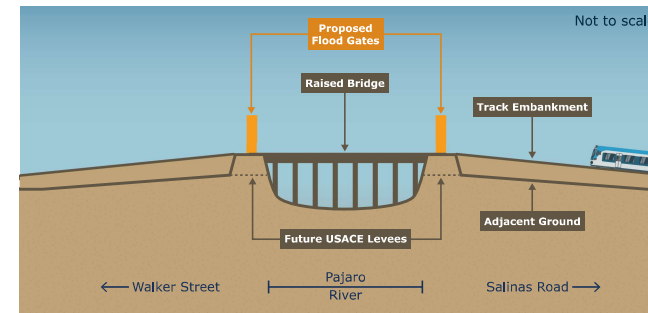


Future Conditions

With the USACE levee project and accounting for climate change, increased precipitation intensities would further raise the water levels in the Pajaro River over time. Sea level rise will have an impact but less than that of the increased precipitation.

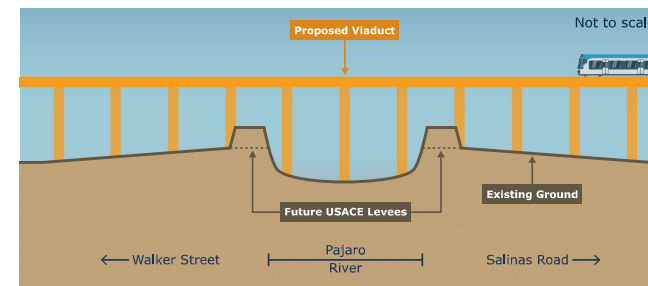
Concepts

What can we do about it?



1 Flood Gates

Flood gates would need to be closed when water levels from precipitation are high. There would be no rail service during those times.



2 Viaduct

Viaduct would need to continue west past Riverside Drive before it could come down to existing grades.

Relocation

3 Bridge crossing moved 1,500 feet southwest of existing rail bridge. Would require relocation of planned Watsonville Station, likely south on Riverside Drive

Flood Gates photo from City of Richmond Virginia.

TO: Elderly & Disabled Transportation Advisory Committee (E&DTAC)
FROM: Nick Danty, Transportation Planner
RE: City of Watsonville Clifford Avenue Road Rehabilitation and Traffic Calming Project

RECOMMENDATION

RTC Staff recommends that the Elderly & Disabled Transportation Advisory Committee (E&DTAC) receive information and provide input on the design of the Clifford Avenue Road Rehabilitation and Traffic Calming Project

BACKGROUND

The Clifford Avenue Road Rehabilitation and Traffic Calming Project will improve the pavement quality, enhance pedestrian and bicycle safety with striping and signage, and implement traffic calming measures to slow down vehicles. The City of Watsonville is in the process of finalizing the design and will present the plans to the June 9th E&DTAC meeting.

DISCUSSION

Clifford Avenue between Main Street and Pennsylvania Drive in Watsonville exhibits poor pavement quality and a lack of bicycle and pedestrian safety facilities. The project will extend the pavement service life by 15-20 years and improve roadway safety by implementing the following major components:

- ADA curb ramps
- Sidewalk repairs
- Class II bike lanes
- High visibility crosswalks
- Corner bulb-outs and new all-way stop at the Montebello intersection; and
- New pavement markings and signage

On-street parking will be removed on the northbound side of the street for the entire length of the project area to create space for the new Class II bike lane and corner bulb-outs. This change is shown in the conceptual cross-section design included in [Attachment 1](#) of this staff report. A more detailed view of the bike lane striping, ADA curb ramp locations, and corner bulb-outs can be found

in the Preliminary Plans contained in Attachment 2.

Funding for this project comes from the following sources

Funding Source	Amount
2025 RTIP Grant Allocation	\$850,000
Redistributed RTIP Funds	\$160,000
City Funds (Measure D, SB1)	\$1,400,000
Total Project Cost (PA&ED, PS&E, and Construction)	\$2,410,000

The City of Watsonville plans to advertise the project for construction bids in Fall 2026 and commence construction in Spring 2027.

Staff recommend that committee members provide input on the draft design to ensure it meets the needs of bicyclists and pedestrians.

SUMMARY

The City of Watsonville will present plans for the Clifford Avenue Road Rehabilitation and Traffic Calming Project for input.

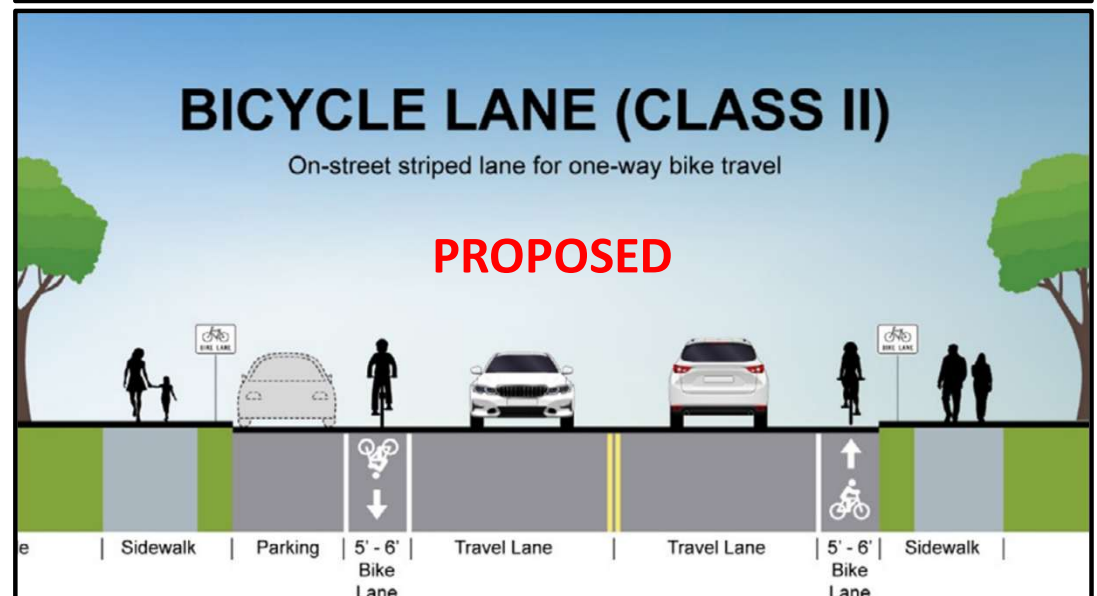
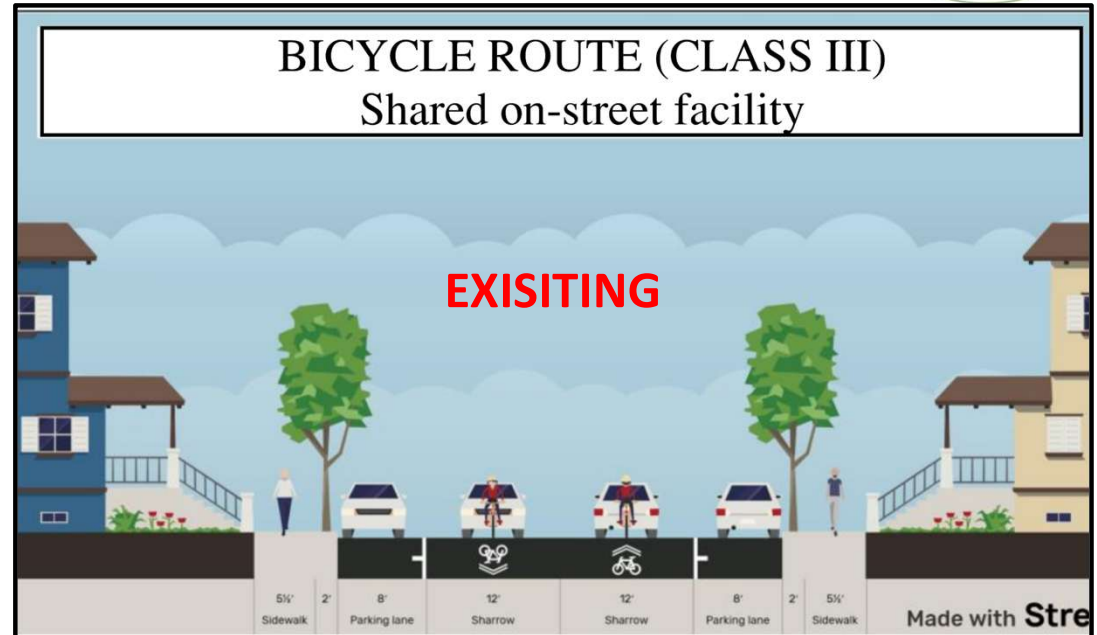
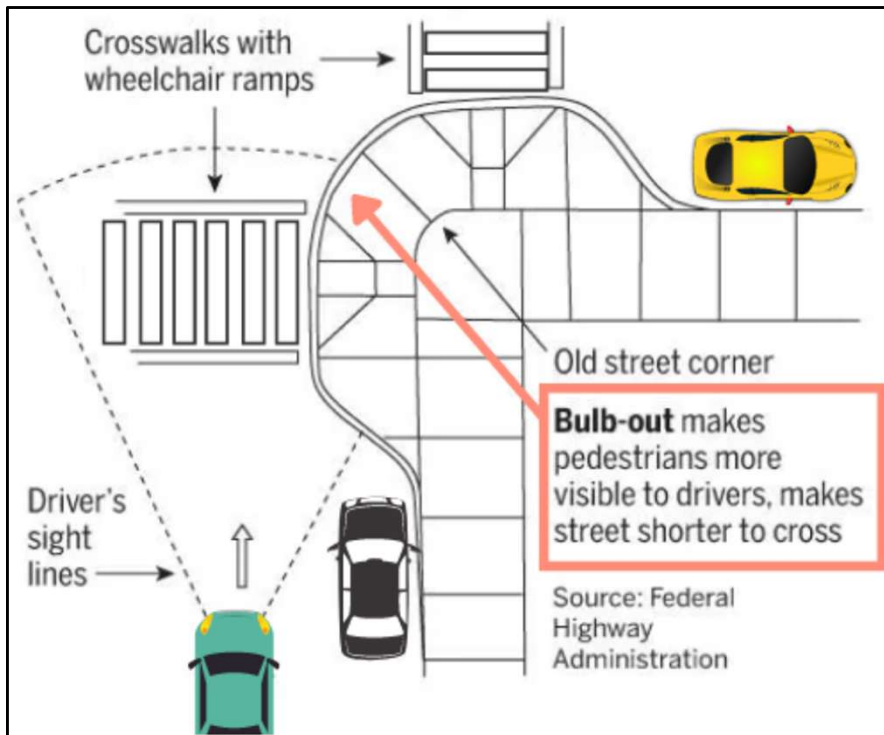
Attachments:

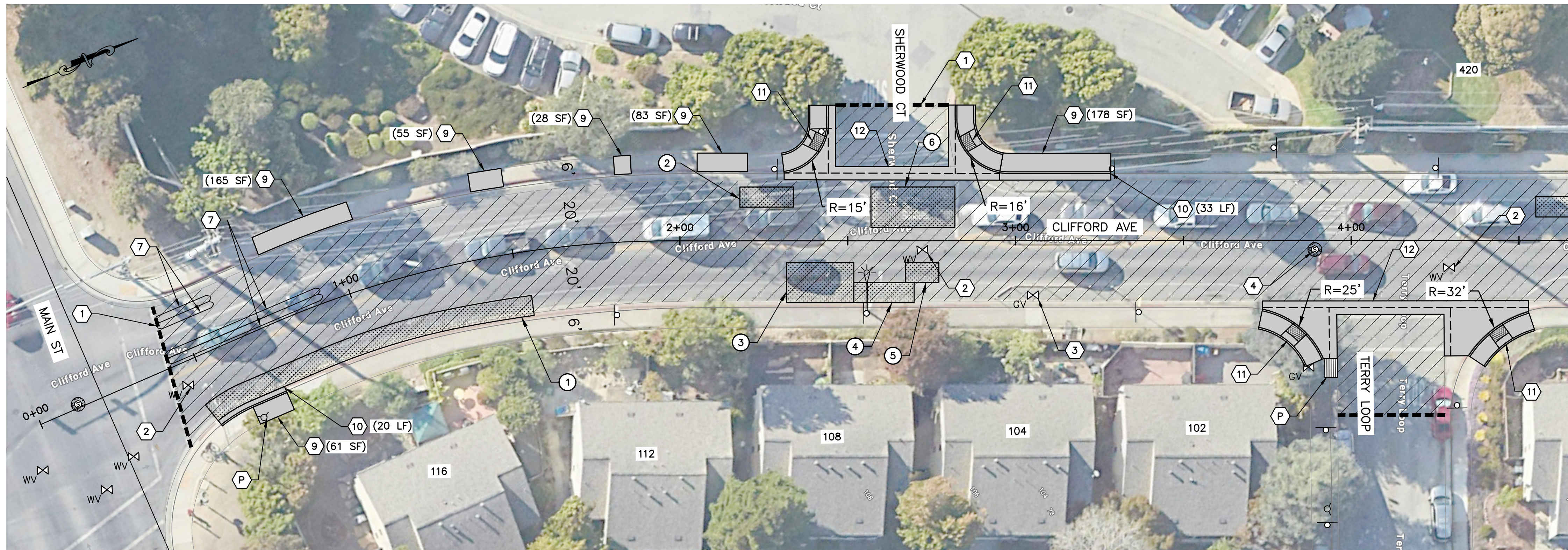
1. Proposed cross-section design
2. Preliminary Plans (excerpts)



Proposed Improvements

- Full roadway rehabilitation
- ADA curb ramp upgrades and sidewalk repairs
- Continuous Class II bike lanes
- High-visibility crosswalks
- Traffic calming improvements
- New all-way stop control at Montebello intersections
- Updated striping and signage





PLAN VIEW
SCALE: 1"=20'

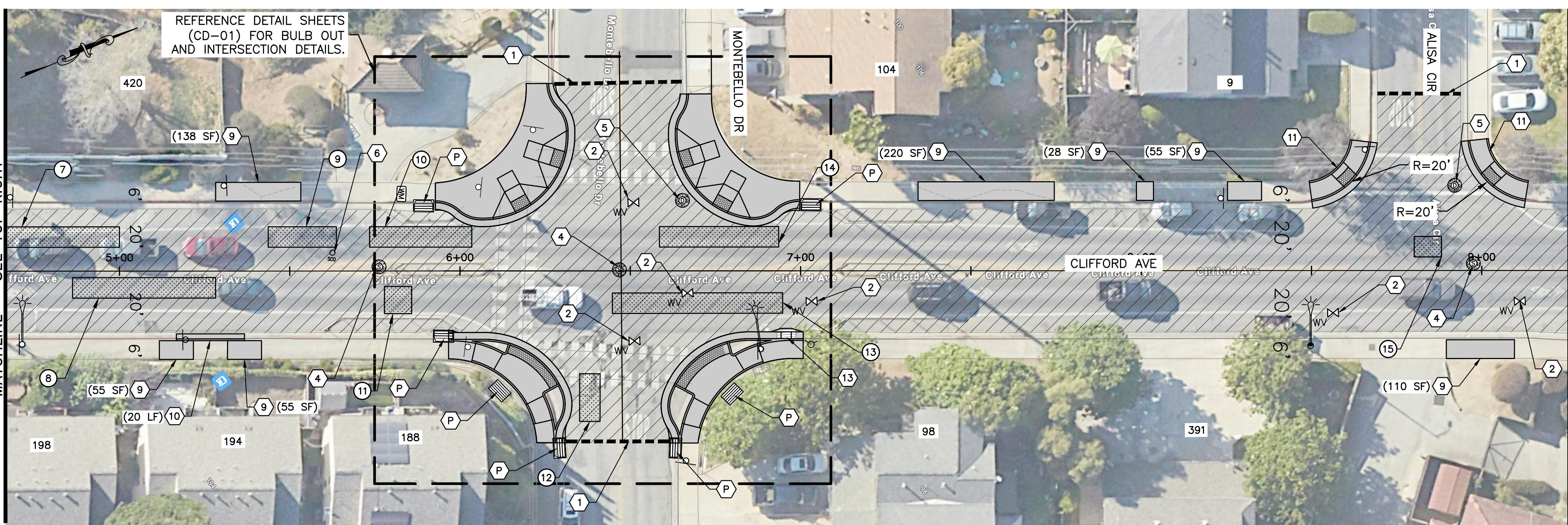
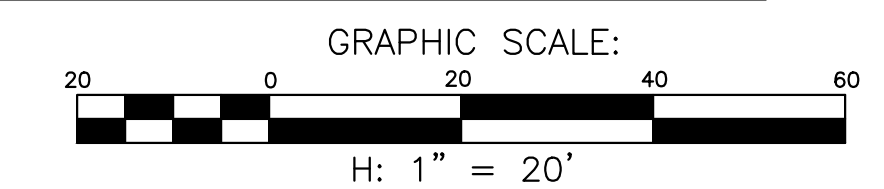
CONSTRUCTION NOTES:

- ① PROTECT IN PLACE.
- ① LIMITS OF PAVEMENT REHABILITATION. EXACT PAVING LIMITS TO BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ② FOR MILL AND INLAY TREATMENTS, THE CONTRACTOR SHALL LOWER ALL EXISTING G05 WATER VALVE BOXES, LID, AND RISERS PRIOR TO MILLING OPERATIONS. THE CONTRACTOR SHALL FURNISH AND INSTALL NEW G05 WATER VALVE BOXES, LID, AND RISERS TO MATCH NEW FINISHED GRADE FOLLOWING PLACEMENT OF ASPHALT PAVEMENT, AND SHALL CONSTRUCT A CONCRETE COLLAR IN ACCORDANCE WITH DETAIL: $\frac{4}{D-01} \frac{6}{D-01}$
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- ⑧ REMOVE AND REPLACE EXISTING SIDEWALK PER DETAIL: $\frac{8}{D-02}$. EXACT LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER PRIOR TO CONSTRUCTION.
- ⑩ REMOVE AND REPLACE EXISTING CURB AND GUTTER PER DETAIL: $\frac{8}{D-02}$. EXACT LOCATION TO BE DETERMINED IN THE FIELD BY ENGINEER PRIOR TO CONSTRUCTION.
- ⑪ REMOVE AND REPLACE EXISTING CURB RAMP WITH "CASE C" CURB RAMPS PER 2025 CALTRANS STD PLAN A88A.
- ⑫ REMOVE AND REPLACE EXISTING CROSS GUTTER PER DETAIL: $\frac{9}{D-02}$
- ⑬ EXISTING CATCH BASIN CONCRETE BOX TO REMAIN. CONTRACTOR TO REMOVE AND REPLACE EXISTING BROKEN CURB AND GUTTER, CATCH BASIN HOOD, AND FRAME GRATE PER DETAIL: $\frac{10}{D-02}$

LEGEND:

- 4-INCH MILL AND INLAY PER DETAIL: $\frac{1}{D-01}$
- DIGOUT NUMBER # PER BASE REPAIR TABLE ON SHEET 2.
- 4-INCH LOCALIZED BASE REPAIRS PER DETAIL: $\frac{2}{D-01}$
- EXACT DIGOUT LOCATIONS TO BE MARKED OUT IN THE FIELD BY THE ENGINEER PRIOR TO THE START OF CONSTRUCTION.
- PROPOSED CONCRETE IMPROVEMENTS PER DETAIL: $\frac{8}{D-02}$

CONTRACTOR MUST REMOVE ALL EXISTING CALMING DELINEATORS ALONG CLIFFORD AVENUE BETWEEN STATION 0+42 AND STATION 21+15 PRIOR TO GRINDING OF EXISTING ROADWAY.



PLAN VIEW
SCALE: 1"=20'

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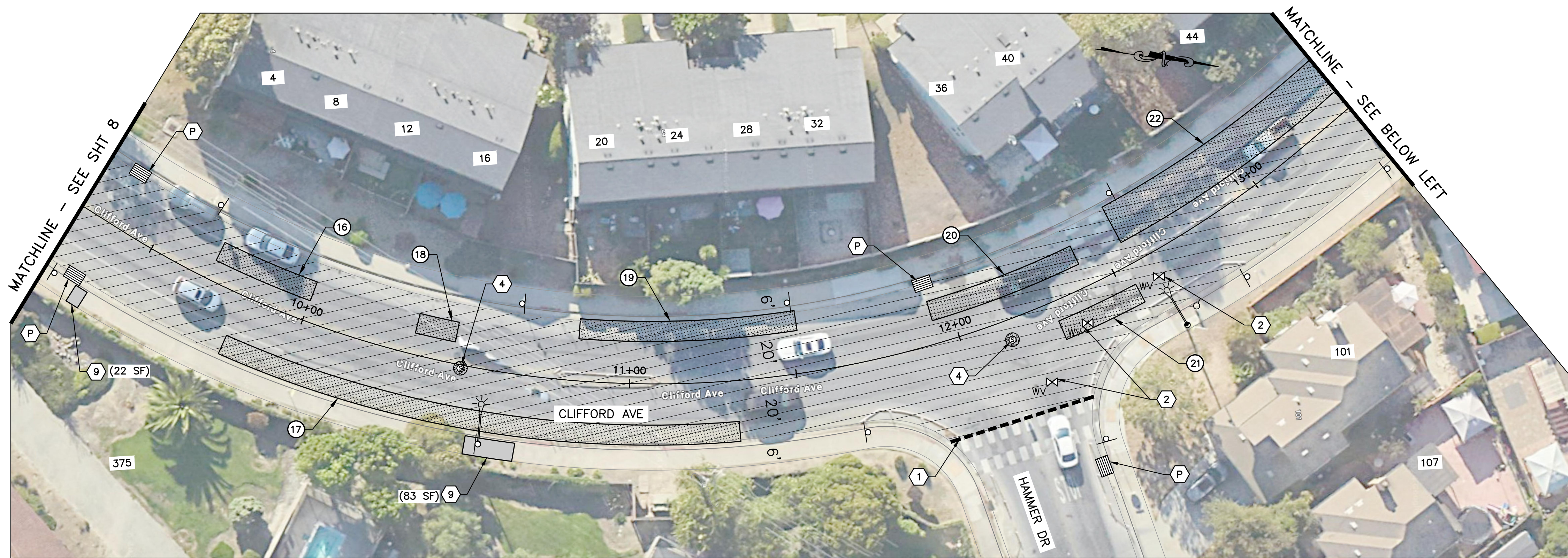
DESIGNED BY: C.M.
DRAWN BY: M.A.
CHECKED BY: R.B.
DATE ISSUED: 05/22/2026
JOB NO.: 1240102.001

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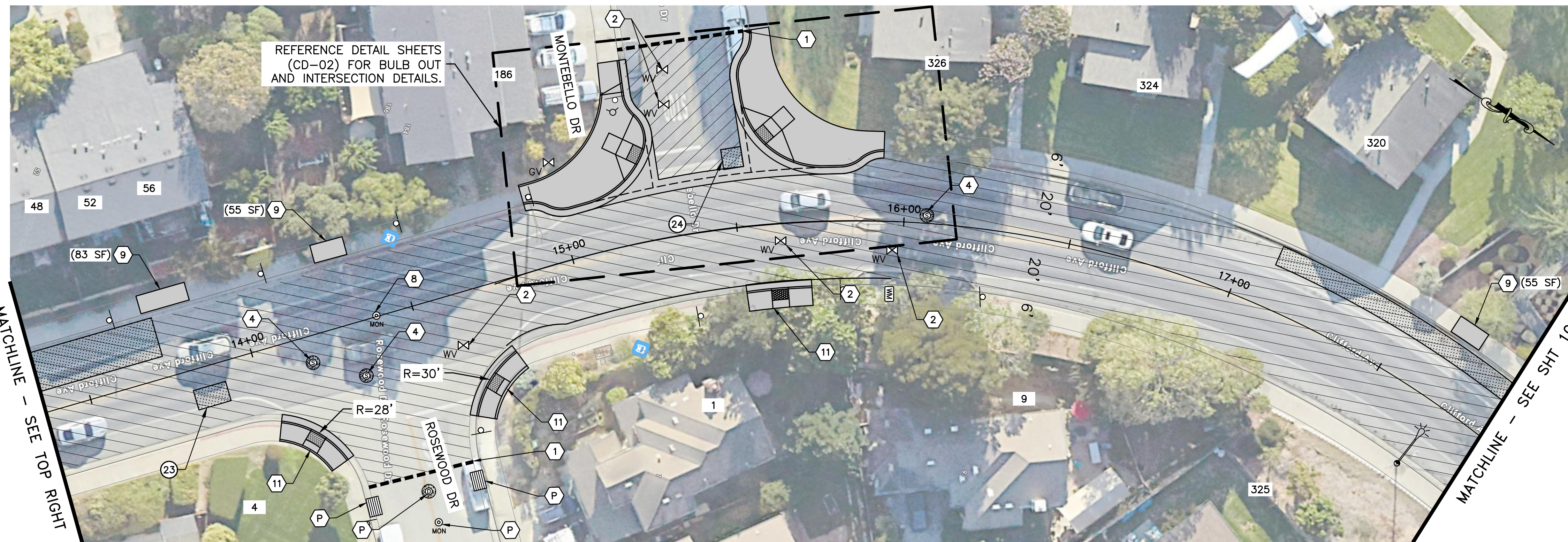
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 SEMI-FINAL
 FINAL
 BID SET
SCALE: AS SHOWN

SHEET TITLE:
CLIFFORD AVENUE - PAVEMENT PLANS
PROJECT TITLE:
CLIFFORD AVENUE ROAD IMPROVEMENTS-PHASE 1 PROJECT #ST-##-####

PROJECT NO.: 1240102.001
SHEET: 8 of 12
DRAWING FILE NO.: P-01



PLAN VIEW
SCALE: 1"=20'



PLAN VIEW
SCALE: 1"=20'

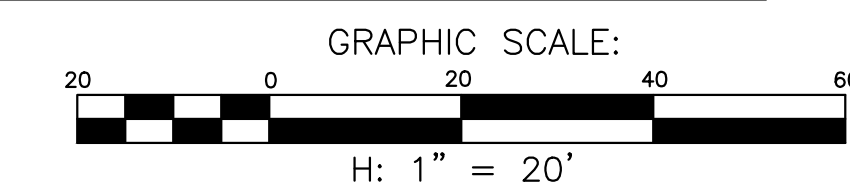
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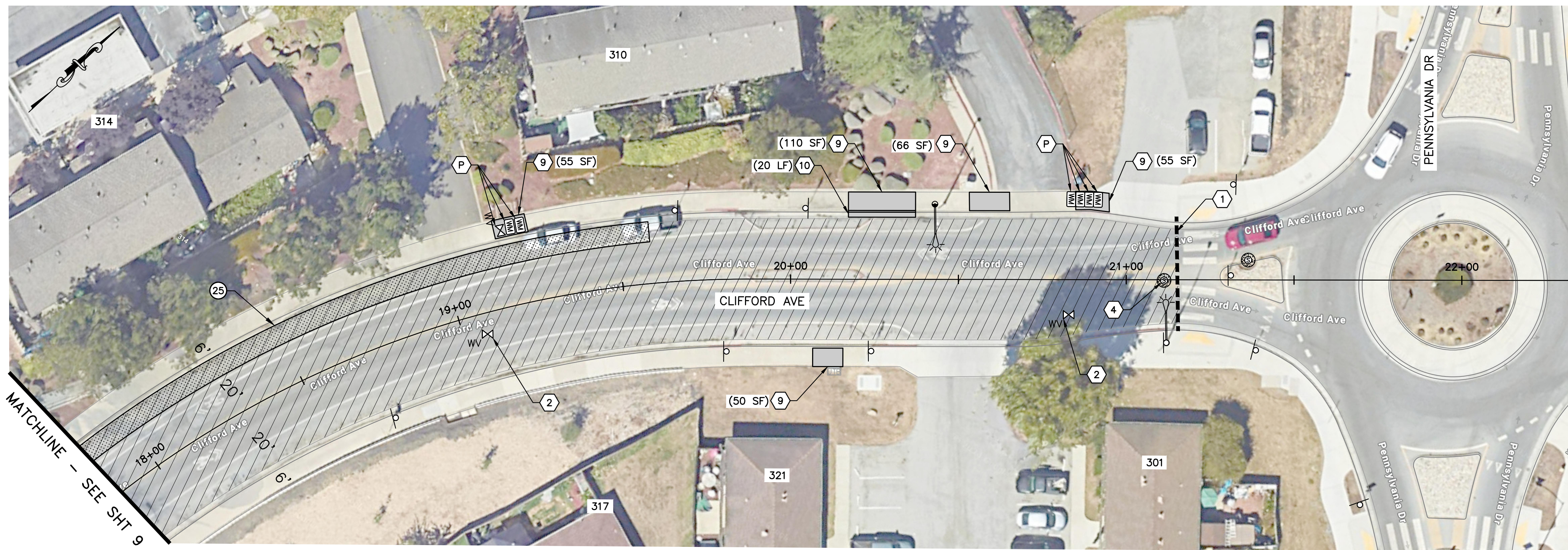
DESIGNED BY: C.M.
DRAWN BY: M.A.
CHECKED BY: R.B.
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SHEET STATUS:
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 FINAL
 BID SET
SCALE: AS SHOWN

SHEET TITLE:
CLIFFORD AVENUE - PAVEMENT PLANS
PROJECT TITLE:
CLIFFORD AVENUE ROAD IMPROVEMENTS-PHASE 1 PROJECT #ST-##-####

PROJECT NO.: 1240102.001
SHEET: 9 of 12
DRAWING FILE NO.: P-02



PLAN VIEW
SCALE: 1"=20'

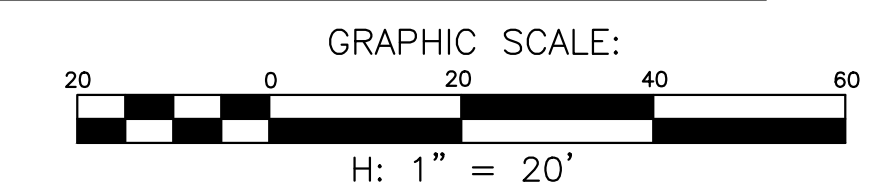
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LEGEND:

- 4-INCH MILL AND INLAY PER DETAIL:
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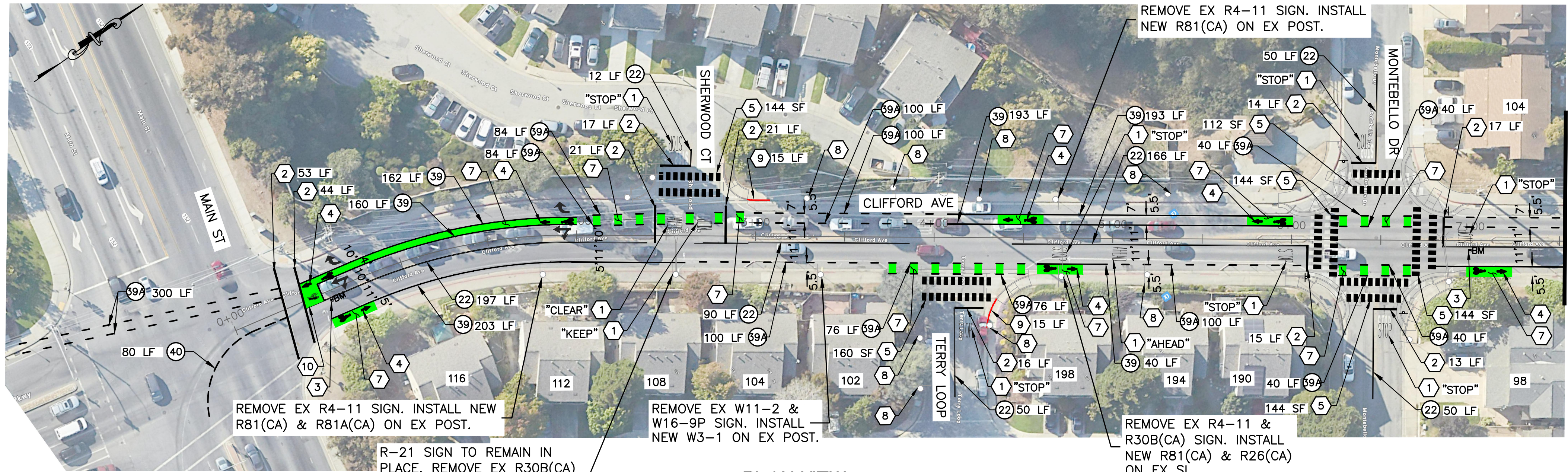
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SHEET STATUS:
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<input checked="" type="checkbox"/> BID SET
SCALE:
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CLIFFORD AVENUE ROAD IMPROVEMENTS-PHASE 1 PROJECT #ST-##-####

PROJECT NO:
1240102.001
SHEET:
10 of 12
DRAWING FILE NO:
P-03



PLAN VIEW
SCALE: 1"=40'

MATCHLINE - SEE BELOW LEFT

GENERAL NOTES:

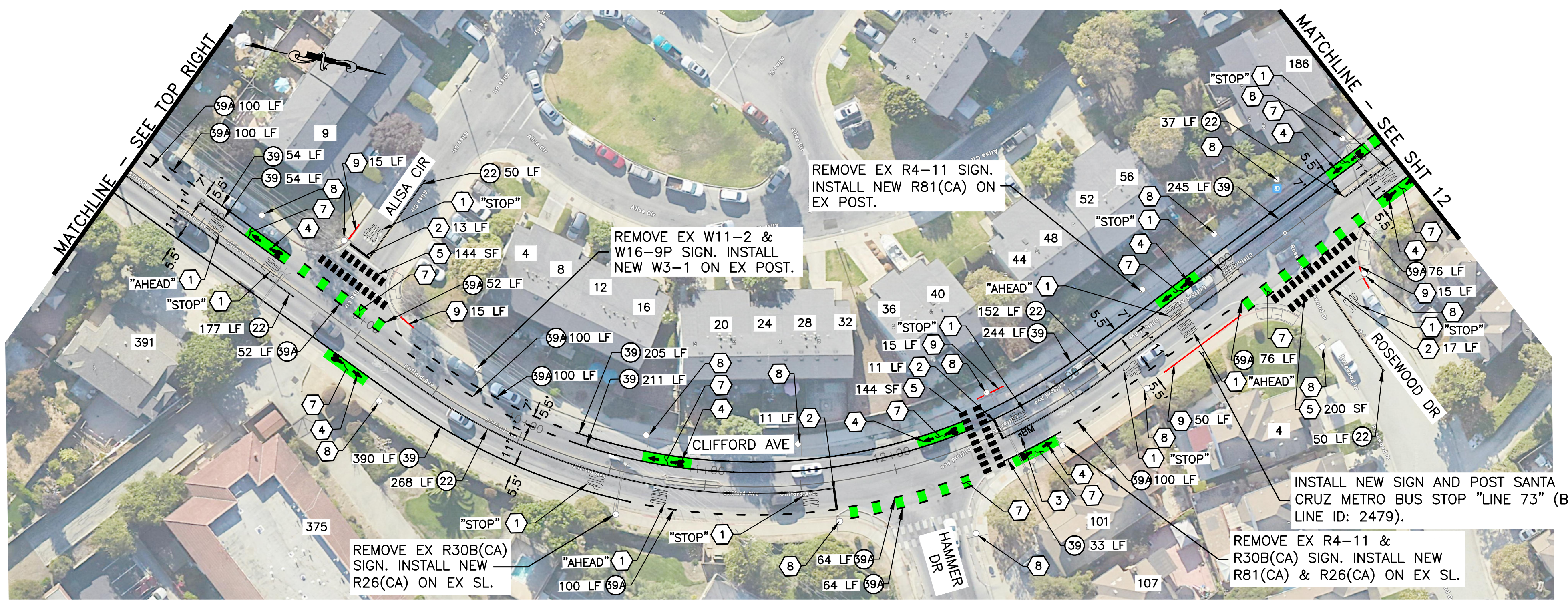
1. ALL CROSSWALK MARKINGS SHALL BE WHITE EXCEPT THOSE NEAR SCHOOLS MUST BE YELLOW.
2. REFER TO SHEETS CD-01 AND CD-02 FOR LOCATIONS OF NEW STOP SIGNS AT CONTROLLED INTERSECTIONS.
3. CONTRACTOR MUST CONFIRM NEW LOCATION OF LINE 73 BUS STOP WITH CITY STAFF AND SANTA CRUZ METRO PRIOR TO THE INSTALLATION OF THE NEW SIGN.

STRIPING NOTES:

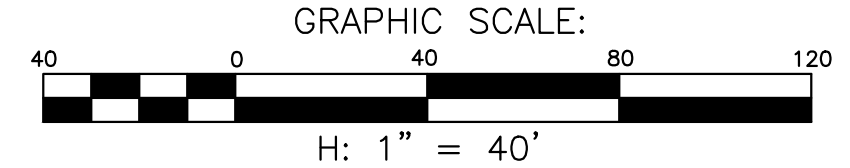
1. INSTALL PAVEMENT MARKINGS LEGENDS PER 2025 CALTRANS STD PLANS. WHITE UNLESS OTHERWISE NOTED ON PLANS. WORDS OR TYPE ARE INDICATED ON PLANS.
2. INSTALL 12" WHITE (W) LIMIT LINES PER 2025 CALTRANS STD PLAN A24G.
3. REPLACE AND/OR INSTALL BLUE, TWO-WAY REFLECTIVE FIRE HYDRANT MARKER PER MUTCD 2026 CALIFORNIA SUPPLEMENT SECTION 3B.11.
4. INSTALL WHITE BIKE LANE MARKINGS WITH ARROW PER 2025 CALTRANS STD PLAN A24A AND A24C.
5. INSTALL HIGH VISIBILITY TRIPLE FOUR CROSSWALK PER DETAIL X ON SHEET D-XX. WHITE UNLESS OTHERWISE NOTED ON PLANS.
6. INSTALL WHITE SHARED ROADWAY BICYCLE MARKING PER 2025 CALTRANS STD PLAN A24C.
7. INSTALL PREFORMED THERMOPLASTIC GREEN-COLORED PAVEMENT MARKINGS FOR BICYCLE FACILITIES PER DETAILS XX-XX ON SHEET D-XX.
8. PROTECT EX STRIPING OR EX SIGN IN PLACE.
9. CONTRACTOR TO PAINT RED CURB
10. INSTALL PREFORMED THERMOPLASTIC GREEN-COLORED PAVEMENT MARKINGS FOR INTERSECTION BICYCLE BOX. REFERENCE 2026 CALIFORNIA MUTCD FIGURE 9E-12.

STRIPING LEGEND:

- TYPE VII (L OR R) ARROW PER CALTRANS STD PLAN A24A.
- TYPE IV (L, R OR B) ARROW PER CALTRANS STD PLAN A24A.
- TYPE I ARROW (10 FT) PER CALTRANS STD PLAN A24A.
- TYPE VI ARROW (10 FT) PER CALTRANS STD PLAN A24A.
- DIMENSION MEASURED ON THE SHEET PANEL WHERE IT IS NOTED (TYP.)
- NUMBER INDICATES PAVEMENT MARKERS AND TRAFFIC LINES DETAIL NUMBER PER 2025 CALTRANS STD PLANS RSP A20A TO A20D.



PLAN VIEW
SCALE: 1"=40'



DRAFT BID SET SUBMITTAL
NOT FOR CONSTRUCTION



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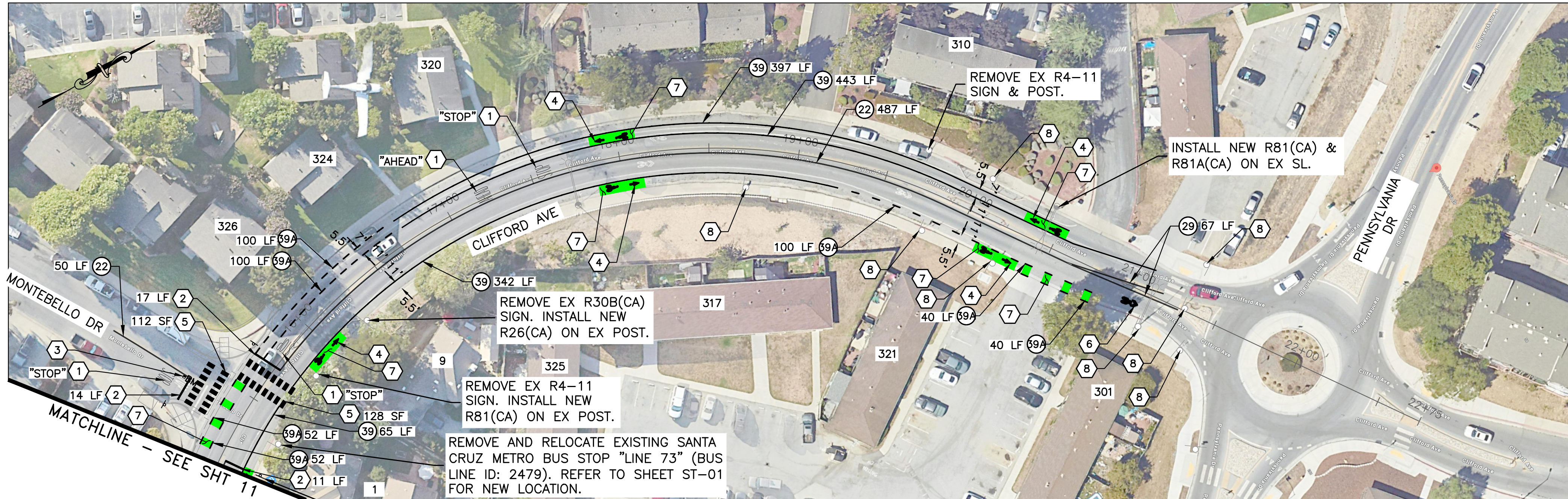
DESIGNED BY: C.M.
DRAWN BY: M.A.
CHECKED BY: R.B.
DATE ISSUED: 05/22/2026
JOB NO.: 1240102.001



SHEET STATUS:
 PRELIMINARY
 SEMI-FINAL
 FINAL
 BID SET
SCALE: AS SHOWN

SHEET TITLE: CLIFFORD AVENUE - SIGNING & STRIPING PLANS
PROJECT TITLE: CLIFFORD AVENUE ROAD IMPROVEMENTS-PHASE 1 PROJECT #ST-##-####

PROJECT NO.: 1240102.001
SHEET: 11 of 12
DRAWING FILE NO.: ST-01



PLAN VIEW
SCALE: 1"=40'

GENERAL NOTES:

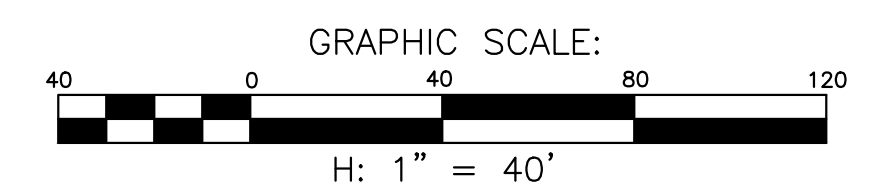
1. ALL CROSSWALK MARKINGS SHALL BE WHITE EXCEPT THOSE NEAR SCHOOLS MUST BE YELLOW.
2. EXISTING TRAFFIC LOOPS TO BE PROTECTED IN PLACE FOR POLYMER MODIFIED SLURRY SEAL TREATMENTS. EXISTING TRAFFIC LOOPS DAMAGED WHILE PERFORMING LOCALIZED BASE REPAIRS (DIGOUTS) MUST BE REPLACED AT THE CONTRACTORS EXPENSE.

STRIPING NOTES:

1. INSTALL PAVEMENT MARKINGS LEGENDS PER 2025 CALTRANS STD PLANS. WHITE UNLESS OTHERWISE NOTED ON PLANS. WORDS OR TYPE ARE INDICATED ON PLANS.
2. INSTALL 12" WHITE (W) LIMIT LINES PER 2025 CALTRANS STD PLAN A24G.
3. REPLACE AND/OR INSTALL BLUE, TWO-WAY REFLECTIVE FIRE HYDRANT MARKER PER MUTCD 2026 CALIFORNIA SUPPLEMENT SECTION 3B.11.
4. INSTALL WHITE BIKE LANE MARKINGS WITH ARROW PER 2025 CALTRANS STD PLAN A24A AND A24C.
5. INSTALL HIGH VISIBILITY TRIPLE FOUR CROSSWALK PER DETAIL X ON SHEET D-XX. WHITE UNLESS OTHERWISE NOTED ON PLANS.
6. INSTALL WHITE SHARED ROADWAY BICYCLE MARKING PER 2025 CALTRANS STD PLAN A24C.
7. INSTALL PREFORMED THERMOPLASTIC GREEN-COLORED PAVEMENT MARKINGS FOR BICYCLE FACILITIES PER DETAILS XX-XX ON SHEET D-XX.
8. PROTECT EX STRIPING OR EX SIGN IN PLACE
9. CONTRACTOR TO PAINT RED CURB.

STRIPING LEGEND:

- TYPE VII (L OR R) ARROW PER CALTRANS STD PLAN A24A.
- TYPE IV (L, R OR B) ARROW PER CALTRANS STD PLAN A24A.
- TYPE I ARROW (10 FT) PER CALTRANS STD PLAN A24A.
- TYPE VI ARROW (10 FT) PER CALTRANS STD PLAN A24A.
- DIMENSION MEASURED ON THE SHEET PANEL WHERE IT IS NOTED (TYP.)
- NUMBER INDICATES PAVEMENT MARKERS AND TRAFFIC LINES DETAIL NUMBER PER 2025 CALTRANS STD PLANS RSP A20A TO A20D.



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DESIGNED BY: **C.M.**
DRAWN BY: **M.A.**
CHECKED BY: **R.B.**
DATE ISSUED: **05/22/2026**
JOB NO.: **1240102.001**



CITY OF WATSONVILLE
PUBLIC WORKS AND UTILITIES
250 MAIN STREET, WATSONVILLE, CALIFORNIA 95076
(831) 768-3117

SHEET STATUS:
 PRELIMINARY
 SEMI-FINAL
 FINAL
 BID SET
SCALE:
AS SHOWN

SHEET TITLE: **CLIFFORD AVENUE - SIGNING & STRIPING PLANS**
PROJECT TITLE: **CLIFFORD AVENUE ROAD IMPROVEMENTS-PHASE 1 PROJECT #ST-##-####**

PROJECT NO.: **1240102.001**
SHEET: **12 of 12**
DRAWING FILE NO.: **ST-02**

TO: Elderly & Disabled Technical Advisory Committee (E&D TAC)
FROM: Nick Danty, Transportation Planner
RE: Cruz 511 Hazard Report Form

RECOMMENDATIONS

Staff recommend that the Elderly & Disabled Technical Advisory Committee receive and review the revised Cruz 511 pedestrian & bicycle hazard report form and provide feedback prior to finalization.

BACKGROUND

The Cruz 511 online pedestrian & bicycle hazard report form is a valuable source of information for the RTC and partner jurisdictions seeking to understand accessibility issues within the countywide bicycle and pedestrian network. Public works departments mostly rely on complaints in order to know when their facilities need attention. The hazard report form is accessible from both the SCCRTC website and from Cruz511.org, where users answer questions and identify the location of the encountered hazard. Over 1,800 bicycle and pedestrian hazards have been reported since the tool was first released to the public in 2011 (figure 1).

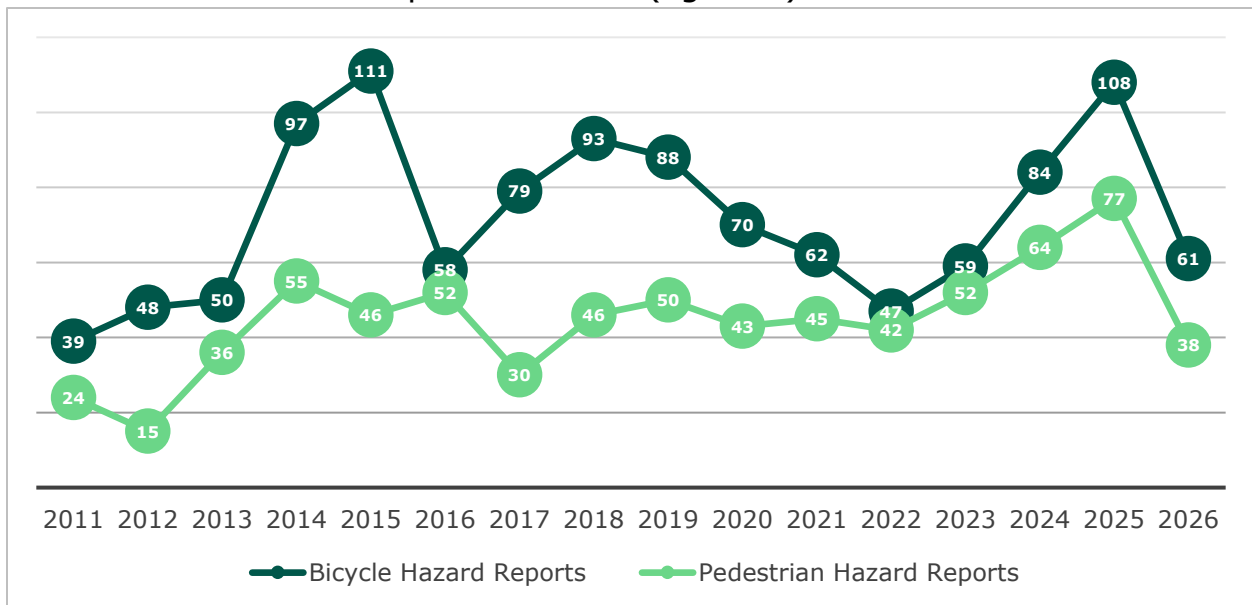


Figure 1. Number of hazard reports received (2011 to 2026)

DISCUSSION

RTC Bicycle Advisory Committee members have voiced concerns over the usability of the current form, citing a difficult user experience on mobile devices. This discourages people from reporting hazards, which gives the false impression that bicycle and pedestrian infrastructure is operating at acceptable levels of service. The intention of the service is to provide one convenient way for people anywhere in Santa Cruz County to report a pedestrian or bicycle hazard and know that it will reach the correct agency quickly. RTC staff have revised the Cruz 511 hazard report form, utilizing the Survey123 platform to improve the appearance of the form on mobile devices by providing a clear format. This should be easier and faster to use, especially when a bicyclist or pedestrian encounters a roadway, sidewalk, or pathway safety hazard and wants to report it right away.

RTC staff are currently in the process of exploring ways to further optimize the hazard report form by including multi-lingual options, automated report generation, and interactive dashboards.

SUMMARY

RTC staff will present the draft revised hazard report form for input.

ATTACHMENTS:

1. [Existing hazard report form](#) (live)
2. [Revised hazard report form](#) (draft)



HAZARD REPORTS

HOW TO SUBMIT A REPORT

If you see something that makes it dangerous for people walking or biking, like a big pothole or a blocked sidewalk, let us know! You can report it anytime using the RTC's Hazard Report. This will help keep our roads and sidewalks safe for everyone. If it's an emergency, call 911 right away.

- 1 Describe the hazard type.
- 2 Describe the hazard location.
- 3 Provide your contact information.

All information submitted becomes part of the public record and may be subject to public disclosure. To notify the RTC about any other hazard (traffic conditions, railway matters, etc), use the [Contact Us](#) form on the homepage.

Watch this webinar for a detailed tutorial on how to submit a hazard report.



Hazard Report Webinar

Regional Transportation Commission



SERVICES

[Bicycle Services](#)

[Transit Services](#)

[Hazard Reports](#)

[Motorist Aid](#)

[Digital Library](#)

[Pedestrian Services](#)

[Seniors & Accessible Services](#)

 [English](#) ▾

REPORT A HAZARD

Hazard Type *(Required)*

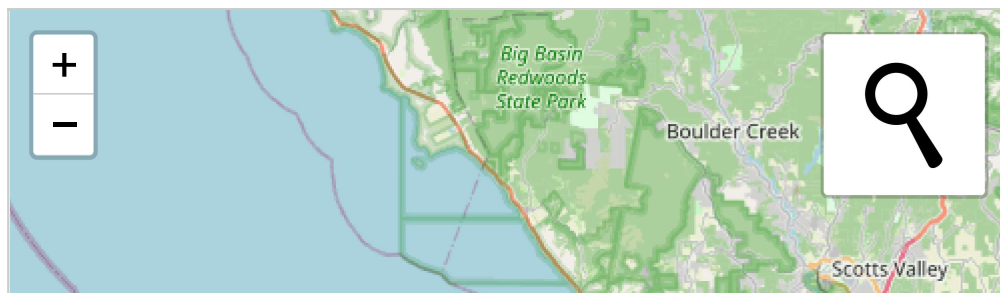
- Bike
- Pedestrian

Describe Hazard *(Required)*

- Rough pavement or potholes
- Pavement cracks
- Lighting problem
- Plant overgrowth or interference
- Traffic signal problem
- Railroad hazard
- Debris on shoulder or bikeway
- Hazardous drain grate
- Bikeway not clearly marked
- Damaged bikeway signs
- Vehicles or objects blocking sidewalk
- Lack of sidewalk
- Debris on sidewalk
- Damaged sidewalk
- Lack of wheelchair access
- Excessive driveway slope
- Sidewalk too narrow
- Pole blocking walkway
- No crosswalk or striping
- Construction hazard
- BCycle electric bikes
- Transit Stop Issues
- Other

FOR IMMEDIATE ENFORCEMENT AND PARKING ISSUES, PLEASE CALL: CHP: (831) 219-0200 (for unincorporated areas) | City of Santa Cruz PD: (831) 471-1131 | Capitola PD: (831) 475-4242 | Scotts Valley PD: (831) 440-5670 | City of Watsonville PD: (831) 768-3300

 English ▾





Location

Describe the hazard location including street name, cross street, landmarks, etc.

Additional Details

Provide any additional details that may help us find and remove the hazard.

Date

Provide the date you encountered this hazard.

mm	dd	yyyy
----	----	------

Upload Photo

Drop files here or **SELECT FILES**

Accepted file types: jpg, jpeg, gif, png, heic, Max. file size: 12 MB, Max. files: 1.

Contact Information *(Required)*

English ▾

First

Last

Email *(Required)*

Phone

Contact Preference *(Required)*

- Phone
- Email
- Do not contact

NOTE: Private property owners are responsible for the maintenance of sidewalks adjacent to their property, per California law. Hazard reports for these conditions will be forwarded to the property owner. See [2010 Report on Sidewalk Safety and Accessibility](#)

I'm not a robot reCAPTCHA

SUBMIT FORM

SUBSCRIBE TO OUR MAILING LIST

Latest news and up-to-date info delivered to your inbox

 SIGN UP

 English ▾

Hazard Report Form



Santa Cruz County Regional Transportation Commission

Report hazards affecting bicycle and pedestrian infrastructure in Santa Cruz County. Please provide accurate details to help us address the issue efficiently.

Hazard Type*

Bike

Pedestrian

Choose Hazard Type*

FOR IMMEDIATE ENFORCEMENT AND PARKING ISSUES, PLEASE CALL: CHP: (831) 219-0200 (for unincorporated areas) | City of Santa Cruz PD: (831) 471-1131 | Capitola PD: (831) 475-4242 | Scotts Valley PD: (831) 440-5670 | City of Watsonville PD: (831) 768-3300


-Please select- ▼

Set Location on Map

Find address or place

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-
Home
Location
Full Screen

Map Controls



Esri, NASA, NGA, USGS, FEMA | Esri Community Maps Contributors, County of Santa Cla... Powered by [Esri](#)

Lat: Lon:

Location Details

(Optional) - For additional location details if needed

1000

Additional Details

1000

Date

▼

Upload Photo

Drop image here or select image

Contact Information ▼

Full Name*

Email*

Phone

Contact Preference*

Phone

Email

Do not contact

NOTE:

Private property owners are responsible for the maintenance of sidewalks adjacent to their property, per California law. Hazard reports for these conditions will be forwarded to the property owner. [See 2010 Report on Sidewalk Safety and Accessibility.](#)

Please enter the characters (not case-sensitive) you see below to submit the survey.



Date	First Name	Last Name	Location	Cross Street	City	Category	Additional Comments	Forwarded To	Forwarded Date	Maintenance Number	Response	Follow-Up
06/01/26	Greg	Giessow	Clares St	40th St	Capitola	Ped: Other	Signpost's post brackets cutoff above sidewalk is broken. Currently, it is causing a trip hazard	Kailash Mozumder	06/01/26		06/01/26 Kailash Mozumder: Thank you for your message, I am away from the office, returning June 4th.	
05/26/26	Jean	Brocklebank	2024 Felt St		Live Oak	Ped: Debris on Sidewalk	The sidewalk all along the front of the property at 2024 Felt Street is partially covered by vegetation, so when pedestrians meet, one has to step into the street. The photo shows just one portion of the sidewalk in front of 2024 Felt	DPW	05/27/26		05/27/26 Brittni Smrz: Good morning Daniel & Jean, Thank you for submitting a bicycle hazard report. I will forward to our Encroachment division for review.	
05/22/26	Kevin	Lockwood	Plateau Ave	Woodrow Ave and Alamar Ave	Santa Cruz	Ped: Plant Overgrowth or Interference	N/A	Dan Estranero, Joanna Edmonds	05/26/26		05/26/26 Joanna Edmonds: Hi Kevin, Thank you for reporting your concerns. Could you give me an idea of what part of Plateau between Woodrow and Almar this is on? If you don't have the exact address, letting me know which side of Bethany Curve would help narrow it down.	
05/19/26	Barry	Scott		State Park Drive and Mar Vista Drive	Seacliff	Ped: Plant Overgrowth or Interference	Very thick and tall overgrowth of dry grass and bushes creating a fire hazard and rodent habitat. I was asked by resident Judy Wygant what to do about the problem behind her home.					
05/16/26	Jean	Brocklebank	1019 Brommer St		Live Oak	Ped: Debris on Sidewalk	On May 1, the date of the attached picture, we observed a new pile of gravel that will be spread on the steep driveway. We request that either the property owner or the renter keep the sidewalk swept clean of the gravel. The gravel makes it difficult to roll our shopping basket.	DPW	05/18/26		05/18/26 Arizza Murillo: Hello, Thank you for your email. I am forwarding to our Road Maintenance division for review & response.	
05/15/26	Rita	Hester	High St		Santa Cruz	Ped: Plant Overgrowth or Interference	Just past the bus stop 2090, route #11. Approx 50 feet from intersection of High Street and Cardiff, and the cross walk that is there. This has been here for a couple of years, but now it really needs attention. THANK YOU!!	Dan Estranero, Joanna Edmonds	05/18/26			
05/11/26	Rebecca	Downing	Center Ave	East St	Seacliff	Ped: Other	Cars park right next to the crosswalk blocking the site lines. Pedestrians must step out into the street around the vehicles into the traffic lane and cars cannot see them coming. The sidewalk next to the crosswalk needs red paint so this location is daylighted.	DPW	05/12/26		05/12/26 Brittni Smrz: Good morning Rebecca & Daniel, Thank you for submitting a bicycle hazard report. I will forward to our Traffic division for review.	
05/07/26	Ehrin	Meijer	Soquel Dr	Mission Dr	Soquel	Ped: Other	The electrical panel at the base of the light/crosswalk pole has been removed. It is currently exposing wires inside. The panel previously had the cover loosely held in place by tape, and this has eventually fallen off.	DPW	05/11/26		05/11/26 Arizza Murillo: Hello, Thank you for your email. I am forwarding to our Traffic division for review & response.	